



## **CITY OF KIRKLAND**

**Planning and Community Development Department**  
123 Fifth Avenue, Kirkland, WA 98033 425.587.3225  
[www.kirklandwa.gov](http://www.kirklandwa.gov)

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### **DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT**

### **DESIGN RESPONSE CONFERENCE**

### **STAFF ANALYSIS**

**File No:** DRC12-00001  
**Project Name:** Transit Oriented Development at South Kirkland Park and Ride  
**Applicant:** Polygon NW  
**Project Planner:** Janice Coogan, Senior Planner  
**Date:** March 26, 2012  
**Meeting Date and Place:** 7:00 p.m., April 2, 2012  
City Hall Council Chambers  
123 5<sup>th</sup> Avenue, Kirkland

### **I. RECOMMENDATION**

Approve subject to the following conditions:

1. This application is subject to the applicable requirements contained in the KMC, Kirkland Zoning Code (KZC), Fire and Building Code, and Public Works Standards. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 1, Development Standards, is provided to familiarize the applicant with some of the additional development regulations.
2. A site plan and building elevations consistent with the plans approved by the DRB.
3. A final landscaping plan consistent with the landscape plan approved by the DRB.
4. Proposed building materials and colors consistent with those approved by the DRB.
5. Any additional revisions required by the DRB at the Design Response Conference.

April 2 is the last scheduled meeting for the proposed Transit Oriented Development (T.O.D.) project at the South Kirkland Park and Ride. The remaining issues to be addressed from the March 19, 2012 meeting are discussed in Section III.

### **II. BACKGROUND**

April 2 will be the third Design Response Conference the Board has held on the project (previous meetings February 13, 2012 and March 19, 2012). The Conceptual Design Conference was held on January 9, 2012. The staff memo and project plans for the previous meetings can be found at the following website:

[http://www.kirklandwa.gov/depart/Planning/DRB\\_Meeting\\_Information.htm](http://www.kirklandwa.gov/depart/Planning/DRB_Meeting_Information.htm)

### III. **DESIGN RESPONSE- REMAINING ITEMS FOR DISCUSSION**

At the last Design Response Conference the DRB provided feedback to the applicant on the project and asked for additional information to be provided at the April 2, 2012 meeting. Below is a summary of the Board's discussion by topic area, the additional items they requested the applicant respond to at the next meeting and the applicant's design team's response to those comments.

Attachment 2 is a narrative from Weber Thompson representing the design team responding to the DRB comments. Attachment 3 is the revised plans.

#### A. Imagine Housing Building

For the Imagine Housing building, the focus of the discussion was on the façade facing NE 38<sup>th</sup> PI. The applicant provided three design options for changing the middle portion of the building. With one exception, the DRB agreed with the applicant's preferred design option which includes a block of bay windows with an area for a vertical sign. The Board liked the two toned paint color with the lighter color on the top fourth level shown in Option 1 and requested the applicant to consider making that change. The Board wanted to know the size of façade panels for the bays at the next meeting.

One of the key design issues is how the base of the building and parking structure along the street meets the design guidelines. The Board expressed support for the revised design of the building along the street showing the glassed in recreation/retail/amenity space with steel canopy against the sidewalk, the recessed first floor with downward lighting, and façade treatment of formed board concrete and landscaping at the base.

The Board concluded that the building meets the design guidelines related to upper story step backs along NE 38<sup>th</sup> PI with the use of change in building color, the column of bay windows and the narrow width of the building façade along the street. Overall the Board likes how the building provides the vertical modulation, variation of building materials and design at the pedestrian level.

#### Design Team Response

Attachment 3 incorporates the applicant's preferred design and the DRB comments for the Imagine Housing façade including the three toned bay windows in lighter colored panels, the lighter band of color at the top level, darker paint in the middle section, vertical signage and board formed textured concrete for the base and landscaping along the street. The revised plans call out the building materials and colors including the use of a vertical wood panel system for the elevator/stair tower along the sides of the building to the rooftop terrace.

#### B. Front Yard Setback

The Board discussed the degree that the two buildings along NE 38<sup>th</sup> PI were designed to meet the design guidelines related to pedestrian orientation and treatment of parking structures adjacent to the street. YBD 1 zoning requires a 10' front yard setback for parking structures. The Design Guidelines promote pedestrian oriented design at the street level and retail uses along the sidewalk not blank walls.

The Board supports design of the plans along NE 38<sup>th</sup> PI showing the large window tenant spaces at the sidewalk with direct pedestrian access (a portion will be for retail use), the parking garage façade is treated with a textured concrete and landscaping at the base of

the buildings or in terraces. With the revised plans the Planning Director is comfortable supporting a public easement over the sidewalk rather than dedication.

C. Mixed Use Building

The Board discussed in more detail the design of the mixed use building along NE 38<sup>th</sup> Pl, at the gateway plaza and the transit side of the building. For the April 2 meeting the Board requested the applicant show how the corner of the building above the gateway plaza can be made more dramatic and more detail as viewed from 108<sup>th</sup> Avenue.

Design Team Response

Attachment 2 explains the changes they've made to the corner of the building above the gateway plaza. Attachment 3 shows the gateway corner of the building revised to incorporate wood composite panels along with the two paint colors and a deck to each of the three levels. These changes are more visible from the view angles along 108<sup>th</sup> Avenue.

D. Gateway Area

The DRB supports the design for the gateway plaza and choice of landscaping. The Board likes the choice of landscaping and columnar trees for the opposite side of the driveway on 108<sup>th</sup> Avenue. The Board requested information on the maintenance and durability for the materials to be used for the wood slatted tower, the wood benches around the gateway plaza planter, and if the stairs leading from the plaza to the sidewalk could be widened or the landscaped retaining wall reduced.

Design Team Response

In Attachment 2, the Design Team explains that they would like to keep the stairs as designed because of the challenges with the grade change from the sidewalk to the gateway plaza. The wood benches will be made out of IPE which is a 50 year product of resistance to decay and moisture. The vertical wood slatted tower feature is made from a composite product with a wood grain and carries a 15 year warranty and expected to exceed the warranty timeframe.

E. Midblock Pedestrian Plaza

The DRB requested refined details about the design for the through block pathway regarding materials, and landscaping.

Design Team Response

Attachment 3 provides greater detail of the through block pathway including use of:

- Combination of two feet by 2 feet square precast concrete pedestal pavers and 1 foot x 4 foot precast concrete accent pavers
- Wood benches and concrete seat walls along side landscape planters
- Combination of pedestrian lights, bollards and step lighting
- A palette of trees and seasonal foliage for color, texture, flowers and fragrance.

F. Metro Transit Parking Garage

The DRB discussed the different screening options provided for the two facades of the parking garage and agreed with the applicant's preferred screening option 3. Option 3 shows two densities of perforated aluminum panels both recessed between the horizontal

concrete guardrails and vertical mounted panels on the building's exterior.

Overall the Board supports the proposed design for the transit garage building including the screening panels, the stair tower design, the 12' wide sidewalk in front of the building that connects to the transit center and mixed use building, and design for the base of the building with the textured concrete and landscaping. The stair tower includes a sloped roof with a corrugated metal soffit in an accent color. The same two toned density perforated metal panels are shown on two sides of the stair tower. The Board did point out that it is important to provide some level of transparency in the stair tower for safety concerns.

The Board discussed the proposed landscape plan for hillside surrounding the garage. They recommended not using the Leyland Cyprus because of a shallow root system. They recommended using another variety of tree that results in a similar screening effect and the alders evaluated for removal.

#### *Design Team Response*

In Attachment 2 the Team responded that they share the Board's concerns regarding the transparency/safety concerns for the screen material for the garage and stair tower and are working on a product that will provide the right density or percentage of transparency.

To follow up on the hillside landscaping, the alders have been evaluated by an arborist who recommends not removing them. Instead of the Leyland Cyprus they propose the Hogan Western Red Cedar and/or Weeping Alaskan Cedar.

#### G. Metro Transit Center

The DRB had concerns whether the two bus shelters shown on the plans are adequate for the amount of transit riders and if there were other options for the design of the shelters. The Board requested information from METRO regarding what types of furniture and bus shelters are prescribed vs what flexibility there is in the design for the transit center. King County Metro Transit should indicate if they support the current design for the transit center.

#### *Design Team Response*

The Team responded with the following comments in Attachment 2:

- Two transit shelters are proposed at the two bus stops along the 12' wide loading island (currently there are 3 smaller ones).
- The transit shelter design is shown on Attachment 2 (last page) with an elevation of the shelter Frame F52 which is approximately 7' tall x 17' long x 5' wide.
- No additional funding is available to upgrade the design. The garage elevator lobby and canopy will provide an additional 20'x30' passenger waiting area.

#### H. Building Materials

The DRB requested the building elevation plans call out the proposed building colors, materials and other details.

#### *Design Response Team*

Attachment 3 provides more details of building materials, colors for each elevation. Below is a summary of what is proposed:

- Imagine Housing Materials:
  - Prefinished metal cornice
  - Cement fiber siding painted dark brown, lighter beige
  - Vinyl window system in white
  - Sheet metal reveals
  - Cement fiber siding
  - Cast in place board formed concrete at parking garage podium base
  - Wood siding stained and sealed elevator overrun and extends along sides of Imagine Housing Building at two locations
- Market Rate Housing Materials:
  - Building façade: Combination of Lap siding, panel siding in four colors (blue, copper, dark brown and gray) and wood composite panels
  - Perforated aluminum screen material for a cornice at sections of top floor
  - Brick Masonry at the base and columns at retail arcade

#### **IV. ATTACHMENTS**

1. Development Standards
2. Applicant's Narrative Response to DRB comments from 3/18/2012
3. Revised Project Plans received March 23, 2012

#### **V. PARTIES**

Applicant's project team  
Department of Planning and Community Development  
Department of Public Works  
Department of Building and Fire Services



CITY OF KIRKLAND  
123 FIFTH AVENUE, KIRKLAND, WASHINGTON 98033-6189 (425) 587-3225

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Date: 2/4/2012

DEVELOPMENT STANDARDS

CASE NO.: DRC12-00001  
PCD FILE NO.: DRC12-00001

**\*\*FIRE DEPARTMENT CONDITIONS\*\***

Additional hydrants are required.

A fire sprinkler system and fire alarm system are required. Depending on the height of the building, a standpipe may also be required.

Available fire flow in the area is approximately 2500 gpm. Fire flow requirement will be based on the size of the building and type of construction.

**\*\*\*BUILDING DEPARTMENT CONDITIONS\*\*\***

Building permits must comply with the 2009 International Building, Mechanical Codes and Uniform Plumbing Code as adopted and amended by the State of Washington and the City of Kirkland.

Structures must comply with 2009 Washington State Energy Code.

Structures must be designed for seismic design category D, wind speed of 85 miles per hour and exposure B

Geotechnical report required to address development activity. The report must be prepared by a Washington State licensed Professional Engineer. Recommendations contained within the report shall be incorporated into the design of the subsequent structures.

You can review your permit status and conditions at [www.kirklandpermits.net](http://www.kirklandpermits.net)

**PUBLIC WORKS CONDITIONS**

Permit #: DRC12-00001  
Project Name: South Kirkland Park and Ride TOD  
Project Address: 3801 108th Ave. NE  
Date: February 3, 2012

Public Works Staff Contacts  
Land Use and Pre-Submittal Process:  
Rob Jammerman, Development Engineering Manager  
Phone: 425-587-3845 Fax: 425-587-3807  
E-mail: [rjammer@ci.kirkland.wa.us](mailto:rjammer@ci.kirkland.wa.us)

Public Works Staff Contacts  
Land Use and Pre-Submittal Process:  
John Burkhalter, Development Engineer Supervisor  
Phone: 425-587-3846 Fax: 425-587-3807  
E-mail: [jb Burkhalter@kirklandwa.gov](mailto:jb Burkhalter@kirklandwa.gov)



## General Conditions:

1. All public improvements associated with this project including street and utility improvements, must meet the City of Kirkland Public Works Pre-Approved Plans and Policies Manual. A Public Works Pre-Approved Plans and Policies manual can be purchased from the Public Works Department, or it may be retrieved from the Public Works Department's page at the City of Kirkland's web site at [www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us).
2. This project will be subject to Public Works Permit and Connection Fees. It is the applicant's responsibility to contact the Public Works Department by phone or in person to determine the fees. The fees can also be review the City of Kirkland web site at [www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us). The applicant should anticipate the following fees:
  - o Water and Sewer connection Fees (paid with the issuance of a Building Permit)
  - o Side Sewer Inspection Fee (paid with the issuance of a Building Permit)
  - o Water Meter Fee (paid with the issuance of a Building Permit)
  - o Right-of-way Fee
  - o Review and Inspection Fee (for utilities and street improvements).
  - o Traffic, Park and School Impact Fee (paid with the issuance of Building Permit). For additional information, see notes below.
3. All street and utility improvements shall be permitted by obtaining a Land Surface Modification (LSM) Permit, but a complete Building Permit for one of the structures must be applied for before the LSM can be issued.
4. Prior to submittal of a Building or Zoning Permit, the applicant must apply for a Concurrency Test Notice. Contact Thang Nguyen, Transportation Engineer, at 425-587-3869 for more information.
5. Building Permits associated with this proposed project will be subject to the traffic, park, and school impact fees per Chapter 27 of the Kirkland Municipal Code. The impact fees shall be paid prior to issuance of the Building Permit(s).
6. All civil engineering plans which are submitted in conjunction with a building, grading, or right-of-way permit must conform to the Public Works Policy titled ENGINEERING PLAN REQUIREMENTS. This policy is contained in the Public Works Pre-Approved Plans and Policies manual.
7. All street improvements and underground utility improvements (storm, sewer, and water) must be designed by a Washington State Licensed Engineer; all drawings shall bear the engineers stamp.
8. All plans submitted in conjunction with a building, grading or right-of-way permit must have elevations which are based on the King County datum only (NAVD 88).
9. A completeness check meeting is required prior to submittal of any Building Permit applications.
10. The required tree plan shall include any significant tree in the public right-of-way along the property frontage.
11. Prior to issuance of any commercial or multifamily Building Permit, the applicant shall provide a plan for garbage storage and pickup. The plan shall conform to Policy G-9 in the Public Works Pre-approved Plans and be approved by Waste Management and the City.

## Sanitary Sewer Conditions:

1. The existing sanitary sewer main within NE 38th Street is adequate to serve the TOD portion of the project however the sewer is only approximately 9 ft deep in the street and the design engineers will need to account for this depth in their design. Provide six-inch minimum side sewer from the public sewer main in NE 38th Street to the TOD building.



2. The City of Kirkland Public Works Department has had discussions with the Bellevue Utility Department and has agreed that any sewer and water needed for the parking garage can be served by the Kirkland sewer and water system. A Memorandum of Understanding between the two Cities's will be drafted and signed.

3. A six-inch minimum side sewer can serve the parking garage, but the Kirkland sewer main extension must be within 150 ft of the garage. Kirkland Public Works will work with the project civil engineer on this layout and design.

#### Water System Conditions:

1. The existing 8-inch water main within the property will need to be extended/looped to the existing 12-inch main in NE 38th Street the said loop shall be within existing right-of-way or encompassed in a 15 ft wide public utility easement.

2. If Kirkland needs to serve the parking garage with a water service and/or fire line, the domestic water meters shall be sized per the UPC; fire lines are not metered. As mentioned above with sanitary sewer, a Memorandum of Understanding regarding water and sewer service will be developed by the two Cities.

3. Provide water services to the TOD building sized per the UPC. The commercial uses shall have a separate water service(s) from the residential uses.

4. In mixed-use projects each use shall have a separate water meter, i.e., the retail use shall have a separate water meter from residential use.

5. A separate irrigation meter is optional, but recommended.

#### Surface Water Conditions:

##### 2009 KCSWDM

1. Provide temporary and permanent storm water control per the 2009 King County Surface Water Design Manual and the Kirkland Addendum. See Policies D-2 and D-3 in the PW Pre-Approved Plans for drainage review information, or contact City of Kirkland Surface Water staff at (425) 587-3800 for help in determining drainage review requirements. This site qualifies for Full Drainage Review.

#### Full Drainage Review

" A full drainage review is required for any proposed project, new or redevelopment, that will:

" Add or replaces 5,000ft<sup>2</sup> or more of new impervious surface area,

" Propose 7,000ft<sup>2</sup> or more of land disturbing activity, or,

" Be a redevelopment project on a single or multiple parcel site in which the total of new plus replaced impervious surface area is 5,000ft<sup>2</sup> or more and whose valuation of proposed improvements (including interior improvements but excluding required mitigation and frontage improvements) exceeds 50% of the assessed value of the existing site improvements.

2. Because this project site is one acre or greater, the following conditions apply:

" Amended soil requirements (per Ecology BMP T5.13) must be used in all landscaped areas.

" If the project meets minimum criteria for water quality treatment (5,000ft<sup>2</sup> pollution generating impervious surface area), the enhanced level of treatment is required if the project is multi-family residential, commercial, or industrial. Enhanced treatment targets the removal of metals such as copper and zinc.

" The applicant is responsible to apply for a Construction Stormwater General Permit from Washington State Department of Ecology. Provide the City with a copy of the Notice of Intent for the permit. Permit Information can be found at the following website:

<http://www.ecy.wa.gov/programs/wq/stormwater/construction/>

o Among other requirements, this permit requires the applicant to prepare a Storm Water Pollution Prevention Plan (SWPPP) and identify a Certified Erosion and Sediment Control Lead (CESCL) prior to the start of construction. The CESCL shall attend the City of Kirkland PW Dept. pre-construction meeting with a completed SWPPP.

" Turbidity monitoring by the developer/contractor is required if a project contains a lake, stream, or wetland.

" A Stormwater Pollution Prevention and Spill (SWPPS) Plan must be kept on site during all phases of construction and shall address construction-related pollution generating activities. Follow the guidelines in the 2009 King County Surface Water Design Manual for plan preparation. Also see Kirkland Storm Water Policy D-12.

3. The storm water detention system shall be designed to Level II standards. Historic (forested) conditions shall be used as the pre-developed modeling condition.

4. Provide a level one off-site analysis (based on the King County Surface Water Design Manual, core requirement #2).

5. When applicable, structural source control measures, such as car wash pads or dumpster area roofing, shall be shown on the site improvement plans submitted for engineering review and approval. Refer to Volume IV in the 2005 Department of Ecology Storm Water Management Manual for Western Washington for further information.

6. Any off-site storm water must by-pass the on-site storm water detention system or accounted for in the design of the detention system.

7. Provide an erosion control report and plan with Building or Land Surface Modification Permit application. The plan shall be in accordance with the 2009 King County Surface Water Design Manual.

8. Construction drainage control shall be maintained by the developer and will be subject to periodic inspections. During the period from May 1 and September 30, all denuded soils must be covered within 7 days; between October 1 and April 30, all denuded soils must be covered within 12 hours. Additional erosion control measures may be required based on site and weather conditions. Exposed soils shall be stabilized at the end of the workday prior to a weekend, holiday, or predicted rain event.

#### Street and Pedestrian Improvement Conditions:

1. The subject property abuts NE 38th Place (in Kirkland). This street is a Collector type street. Zoning Code sections 110.10 and 110.25 require the applicant to make half-street improvements in rights-of-way abutting the subject property. Plate 34 in the Kirkland Zoning Code establishes that this street must be improved with the following:

A. The center turn lane, two through lanes and 5 ft wide bike lanes shall be maintained. If the existing bike lane along the property frontage is less than 5 ft wide, it shall be widened to 5 ft.

B. On-street parking is encouraged. In the area where on-street parking is proposed, the width of the parking lane shall be 8 ft minimum.

C. The sidewalk along the street shall be 10 ft wide behind a 6-inch wide vertical curb and gutter (type A).

D. Street trees planted in 4x6 ft tree wells, 30 ft on center will be required.

E. Pedestrian lighting (standard is currently being determined) shall be installed 60 ft on-center along the property frontage.

F. All public street improvements to the back of the new curb shall be encompassed in dedicated right-of-way. The sidewalk may be encompassed in a public sidewalk easement if approved by the Planning Department.

2. It is recommended, and may be requested as SEPA mitigation, that the project install the missing 150 ft long sidewalk link to the west of the project along NE 38th Street. This project is very pedestrian oriented and people coming to and from the TOD project or the park and ride cannot safely walk along the north side of NE 38th Street due to this missing sidewalk link.



3. The optional garage entry from NE 38th Place will be considered after site distance, turning movements, and queuing are analyzed by the City Transportation Engineer. The Transportation Engineer also raised the possibility of a traffic signal at the driveway on 108th Ave. NE and at the intersection of 108th Ave. NE and NE 38th Street with ITS system linked to the existing traffic signal at 108th Ave. NE and Northup Way. The traffic study will need to review the LOS at these intersections to determine if traffic signals are warranted. The Kirkland Transportation Engineer will be coordinating these issues with the Bellevue Traffic Engineer.
4. A 2-inch asphalt street overlay will be required where three or more utility trench crossings occur within 150 lineal ft. of street length or where utility trenches parallel the street centerline. Grinding of the existing asphalt to blend in the overlay will be required along all match lines.
5. The project should plan to remove and replace all existing curb and gutter along NE 38th Place.
6. All street and driveway intersections shall not have any visual obstructions within the sight distance triangle. See Public Works Pre-approved Policy R.13 for the sight distance criteria and specifications.
7. It shall be the responsibility of the applicant to relocate any above-ground or below-ground utilities which conflict with the project associated street or utility improvements.
8. Underground all new and existing on-site utility lines and overhead transmission lines.



## CITY OF KIRKLAND

Planning and Community Development Department

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[www.kirklandwa.gov](http://www.kirklandwa.gov)

## Planning and Community Development Standards

### LAND SURFACE MODIFICATION AND/OR BUILDING PERMIT REQUIREMENTS

**KZC 56.08-10 YBD 1 Use Zone Chart.** – The applicant must comply with the requirements of this section including the affordability requirements.

In addition, staff has identified the following improvements that need to be installed associated with the project:

- Install a sidewalk along the NE 38<sup>th</sup> PI property frontage located to the north that connects with the new sidewalks to be installed with the project to provide pedestrian connectivity to the TOD
- Install a lighted mid block crosswalk along NE 38<sup>th</sup> PI in a location approved by the City to mitigate for anticipated increased pedestrians traveling to and from the TOD

**KMC 22.20 Subdivisions.** See this section for the short plat requirements and process. The applicant will need to meet the minimum Building Code setbacks from property lines for openings in structures and/or create “no building easements” easements along property.

**KMC 27.06.030 Park Impact Fees.** New residential units are required to pay park impact fees prior to issuance of a building permit. Please see KMC 27.06 for the current rate. Exemptions and/or credits may apply pursuant to KMC 27.06.050 and KMC 27.06.060. If a property contains an existing unit to be removed, a “credit” for that unit shall apply to the first building permit of the subdivision.

**KZC 95.30(4) Tree Protection Techniques.** A description and location of tree protection measures during construction for trees to be retained must be shown on demolition and grading plans.

**KZC 95.34 Tree Protection.** Prior to development activity or initiating tree removal on the site, vegetated areas and individual trees to be preserved shall be protected from potentially damaging activities. Protection measures for trees to be retained shall include (1) placing no construction material or equipment within the protected area of any tree to be retained; (2) providing a visible temporary protective chain link fence at least 6 feet in height around the protected area of retained trees or groups of trees until the Planning Official authorizes their removal; (3) installing visible signs spaced no further apart than 15 feet along the protective fence stating “Tree Protection Area, Entrance Prohibited” with the City code enforcement phone number; (4) prohibiting excavation or compaction of earth or other damaging activities within the barriers unless approved by the Planning Official and supervised by a qualified professional; and (5) ensuring that approved landscaping in a protected zone shall be done with light machinery or by hand.

**KZC 95.40- Required landscaping** -supplemental landscaping

**KZC 95.50 Tree Installation Standards.** All supplemental trees to be planted shall conform to the Kirkland Plant List. All installation standards shall conform to Kirkland Zoning Code Section 95.45.

**KZC 110.60.5 Street Trees.** All trees planted in the right-of-way must be approved as to species by the City. All trees must be two inches in diameter at the time of planting as measured using the standards of the American Association of Nurserymen with a canopy that starts at least six feet above finished grade and does not obstruct any adjoining sidewalks or

driving lanes.

**KZC 95.52 Prohibited Vegetation.** Plants listed as prohibited in the Kirkland Plant List shall not be planted in the City.

**KZC 105 Parking Area Design, Vehicle and Pedestrian Access and related Improvements-**See requirements for the size of parking stalls, driving aisles; bicycle parking; Through Block pathway details; pedestrian pathways through parking area.

**KZC 105.19 Public Pedestrian Walkways.** The height of solid (blocking visibility) fences along pedestrian pathways that are not directly adjacent a public or private street right-of-way shall be limited to 42 inches unless otherwise approved by the Planning or Public Works Directors. All new building structures shall be setback a minimum of five feet from any pedestrian access right-of-way, tract, or easement that is not directly adjacent a public or private street right-of-way. If in a design district, see section and Plate 34 for through block pathways standards.

**KZC 115.25 Work Hours.** It is a violation of this Code to engage in any development activity or to operate any heavy equipment before 7:00 am. or after 8:00 pm Monday through Friday, or before 9:00 am or after 6:00 pm Saturday. No development activity or use of heavy equipment may occur on Sundays or on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas Day. The applicant will be required to comply with these regulations and any violation of this section will result in enforcement action, unless written permission is obtained from the Planning official.

**KZC 115.45 Garbage and recycling receptacles and enclosure- Placemnt and Screening.**

**KZC 115.59 Height Regulations- Calculating Average Building Elevation (ABE).** See #3 for partially underground structure or improvements.

**KZC 115.95 Noise Standards.** The City of Kirkland adopts by reference the Maximum Environmental Noise Levels established pursuant to the Noise Control Act of 1974, RCW 70.107. See Chapter 173-60 WAC. Any noise, which injures, endangers the comfort, repose, health or safety of persons, or in any way renders persons insecure in life, or in the use of property is a violation of this Code.

**KZC 115.135 Sight Distance at Intersection.** Areas around all intersections, including the entrance of driveways onto streets, must be kept clear of sight obstruction as described in this section.

**KZC 145.22.2 Public Notice Signs.** Within seven (7) calendar days after the end of the 21-day period following the City's final decision on the permit, the applicant shall remove all public notice signs.

<b>PRIOR TO OCCUPANCY</b>
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**KZC 110.75 Bonds.** The City may require or permit a bond to ensure compliance with any of the requirements of the Required Public Improvements chapter.





WEBER THOMPSON



March 23, 2012

Janice Coogan  
City of Kirkland  
123 Fifth Avenue  
Kirkland WA, 98033

Re: Design Response Conference for the proposed Transit Oriented Development at the South Kirkland Park and Ride, File No. DRC12-00001

Dear Mrs. Coogan,

The following narrative is submitted in response to your Letter of March 20, 2012 summarizing the Design Response Conference Board Comments

**Imagine Housing Building**

The board supports the architect's preferred option for the buildings showing the one column of bay windows and vertical sign, except they like the two toned color scheme in Option 1 with the lighter color of paint at the top level.

*Design Team Response: A revised elevation based on the board's recommendation will be submitted under separate cover.*

The board supports the design for the base of the building. As a result I talked to the Planning Director and he is willing to support a public easement over the sidewalk rather than requiring full dedication resulting in the 10' setback for the portions of the parking structures that extend into that area. The details of the easement will be worked out in coordination with the public Works Dept. with the building permit.

*Design Team Response: We will contact Public Works to coordinate the dedication and easement details.*

**Mixed Use Building**

Overall comments were positive especially along the street level façade and use of landscaping. There was a request for the southeast corner of building discussed under the Gateway topic below.

*Design Team Response: See Gateway discussion below.*

**Gateway Plaza**

The board supports the design for the gateway plaza. Because of the prominence of the building corner the design guidelines goals for the gateway area, the board would like to see if the middle portion of the building corner above the gateway could be more dynamic with more interest.

*Design Team Response: The façade above the gateway has been revised as follows: Incorporated more wood composite material to highlight the gateway corner visible*

from both view angles along 108<sup>th</sup> Ave. Additionally, a deck has been added above the gateway to add modulation and interest.

The board would like to see if the stairs leading to the Plaza from the NE 38<sup>th</sup> PI could be opened up and maybe reduce the planter at the corner. For the next meeting they would like more information on the maintenance for the proposed wood benches and vertical wood slatted tower feature.

Design Team Response: We studied this area in detail based on this request. The grade rises/falls rapidly in this area, perpendicular to the stairway. The gradient is steeper than that of the stairs. Because of this slope, widening the stairs would create either more stairs on the low end, or stairs that extend below the grade, or fade into the grade and become a tripping hazard. Because there are many alternate ways for pedestrians to walk into the Gateway area, all around its perimeter of the Gateway Plaza, the design team felt the current width of eight feet is adequate for the amount of pedestrian traffic and in scale with all the other features. We would therefore request to keep the stairs as designed.

The wood benches are proposed to be made out of IPE which is considered a 50-year product for its resistance to decay and moisture. With a Janka rating (measure of wood hardness) of 3680 it is virtually the hardest tradewood available. It has a Grade A fire rating which is the same as concrete. It is extremely difficult to damage, according to the Janka Hardness Scale information. We are also proposing to leave it as a natural finish and allow the wood to go grey with time to coordinate with the building's refined color palate.

The vertical wood slatted tower feature is made from a composite product with wood grain. This type of material carries a 15 year warranty and is not expected to need repair or maintenance within this time frame. The long term durability of the material will most likely exceed the 15 years however this timeframe is set based on the length of time the material has been on the market and tested. As with all buildings owned by Polygon they will provide maintenance, repair or replacement as necessary to maintain a quality development.

### **Transit Parking Garage**

Overall the Board supports the design of the Metro parking garage and proposed landscaping at the base. They agree with the applicant's preferred screening option. At the stair tower, they recommend there be adequate transparency for safety reasons. Regarding the proposed landscaping on the hillside behind the garage, the Board recommends Leyland Cyprus trees not be used because of their shallow root system and the alders be evaluated for safety and removal.

Design Team Response: Metro shares the board and applicants' security and visibility concerns and we are working together to source a screen material that will provide 50-

58% transparency for the low density panel areas. The high density panel areas will remain at a 30% transparency.

The alders on the hillside have been evaluated by an arborist and are not recommended for removal. In lieu of the Leyland Cypress we are proposing the alternative of Hogan Western Red Cedar and/or Weeping Alaskan Cedar.

### **Transit Center**

The Board would like to hear from King County Metro how flexible they are with the design for the bus shelters and other public furniture and if they are in support of what is designed. What other options are there? Could an overhead canopy be used instead of the shelters? They also expressed concern that two shelters are not adequate for the amount of transit riders.

Design Team Response: King County Metro has reviewed and approved the proposed fixture style and locations which rely on their standard specification of furnishings. King County Metro does not have any additional funding for upgrades to the standard furnishings. The furnishings proposed are preferred both from a replacement and maintenance standpoint as they are standards throughout their facilities and promote ease of maintenance. The shelters were specifically selected for their ability to fit within the 12' loading island, maintain bus mirror clearance, and allow additional trees to screen the facility from the adjacent residential units. The shelters shown on the site plan are the F-52 Metro shelter model as indicated in the cut sheet included with the submittal exhibits. The two bus stops each have a 5'x17' shelter. The current bus loading has 3 smaller shelters of a different standard Metro style. In addition to the shelters the garage elevator lobby and its entry canopy provide an additional 20'x30' passenger overflow waiting area.

Sincerely,



Mindy Black  
Senior Associate  
Weber Thompson







# KIRKLAND TOD

Design Response Conference III

APRIL 02, 2012



# IMAGINE HOUSING

## COLOR ELEVATIONS

POLYGON NORTHWEST



KIRKLAND TOD | DESIGN RESPONSE CONFERENCE III



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KIRKLAND TOD | NORTH ELEVATION



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POLYGON NORTHWEST



KIRKLAND TOD | SOUTH ELEVATION

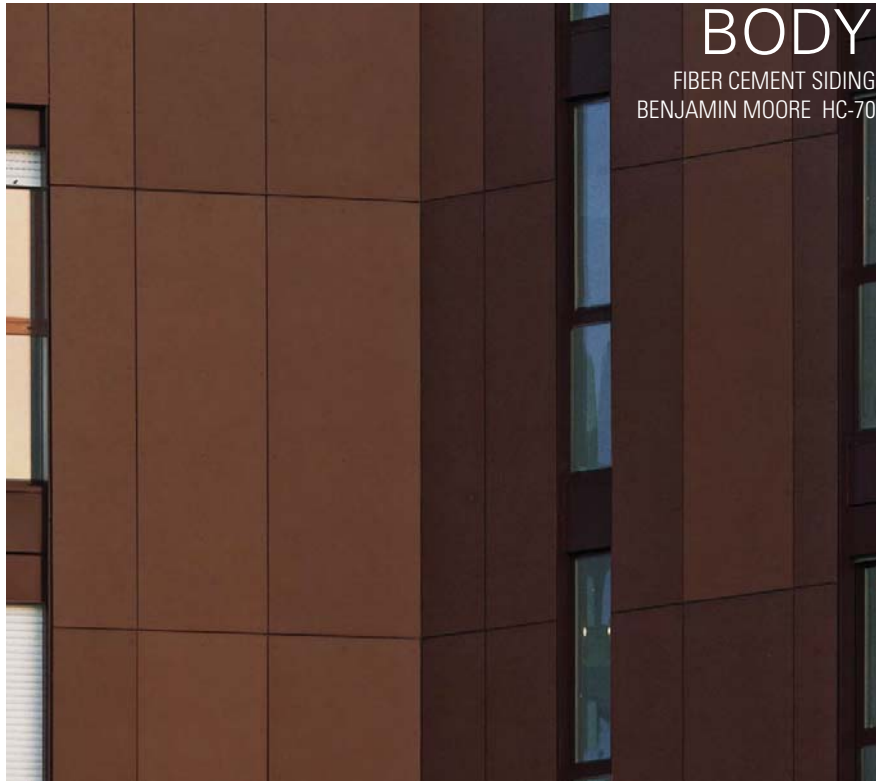


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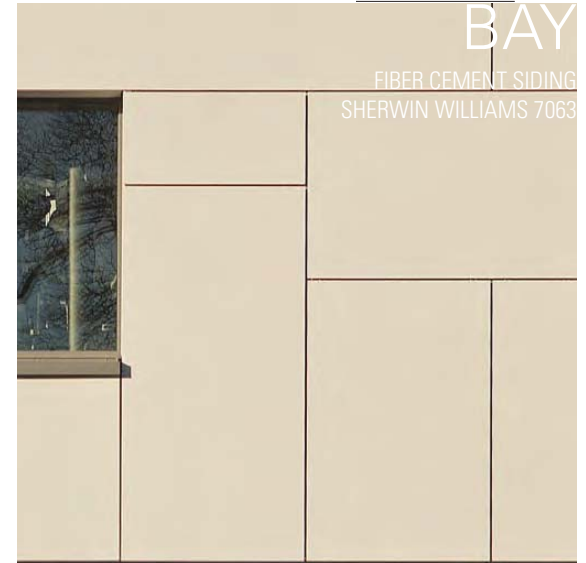
# BODY

FIBER CEMENT SIDING  
BENJAMIN MOORE HC-70



# VERTICAL

WOOD PANEL SYSTEM  
IPE



ATTACHMENT 3

# BAY

FIBER CEMENT SIDING  
SHERWIN WILLIAMS 7063



# BASE

BOARD FORMED CONCRETE  
GRAFFITI RESISTANT SEALER



# ACCENT

FIBER CEMENT SIDING  
SHERWIN WILLIAMS 6003

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KIRKLAND TOD | MATERIAL SELECTIONS



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# MARKET RATE

## COLOR ELEVATIONS



1. Brick Masonry



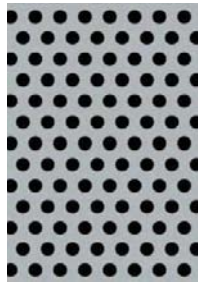
2. Wood Composite Panel



3. Cementitious Lap Siding  
(Lap Exposure 4" or 7")



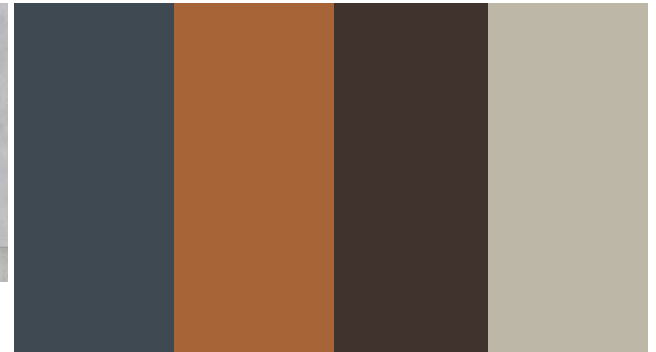
4. Cementitious Panel Siding



5. Perforated  
Aluminum Screen



6. Architectural  
Concrete



A. Blue Note  
BM2129-30

B. Copper Mountain  
SW6356

C. Black Bean  
SW6006

D. Analytical Gray  
SW7051



POLYGON NORTHWEST



KIRKLAND TOD | EAST ELEVATION



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1. Brick Masonry



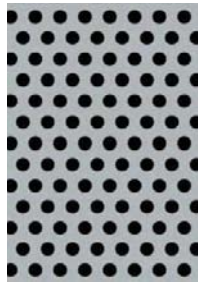
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3. Cementitious Lap Siding  
(Lap Exposure 4" or 7")



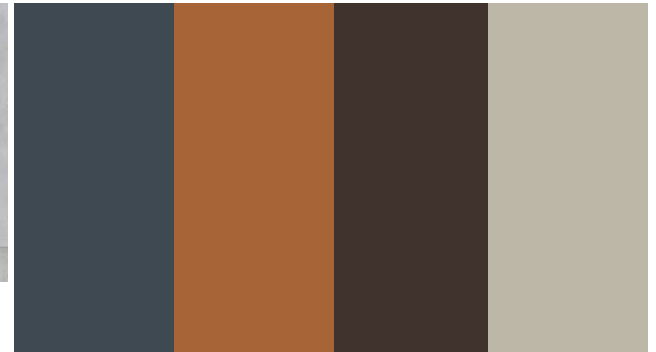
4. Cementitious Panel Siding



5. Perforated  
Aluminum Screen



6. Architectural  
Concrete



A. Blue Note  
BM2129-30

B. Copper Mountain  
SW6356

C. Black Bean  
SW6006

D. Analytical Gray  
SW7051





1. Brick Masonry



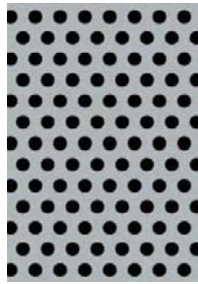
2. Wood Composite Panel



3. Cementitious Lap Siding  
(Lap Exposure 4" or 7")



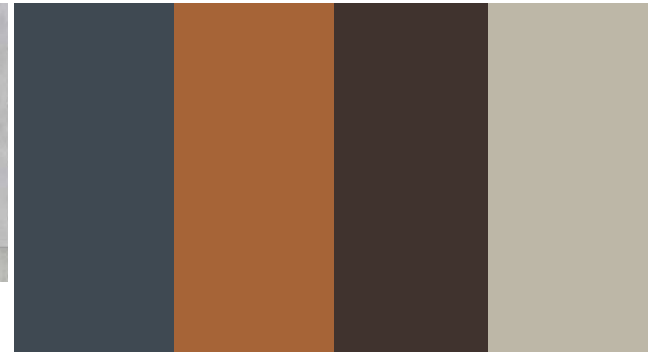
4. Cementitious Panel Siding



5. Perforated  
Aluminum Screen



6. Architectural  
Concrete



A. Blue Note  
BM2129-30

B. Copper Mountain  
SW6356

C. Black Bean  
SW6006

D. Analytical Gray  
SW7051





1. Brick Masonry



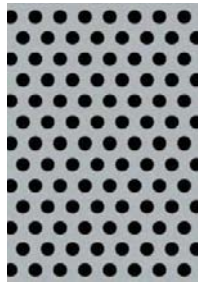
2. Wood Composite Panel



3. Cementitious Lap Siding  
(Lap Exposure 4" or 7")



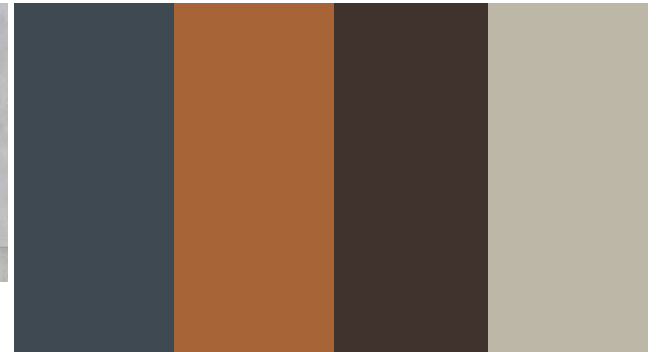
4. Cementitious Panel Siding



5. Perforated  
Aluminum Screen



6. Architectural  
Concrete



A. Blue Note  
BM2129-30

B. Copper Mountain  
SW6356

C. Black Bean  
SW6006

D. Analytical Gray  
SW7051



SOUTH ELEVATION



GATEWAY WEST ELEVATION





1. Brick Masonry



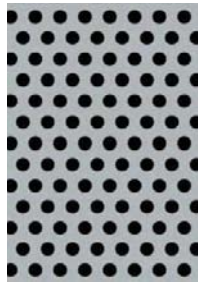
2. Wood Composite Panel



3. Cementitious Lap Siding  
(Lap Exposure 4" or 7")



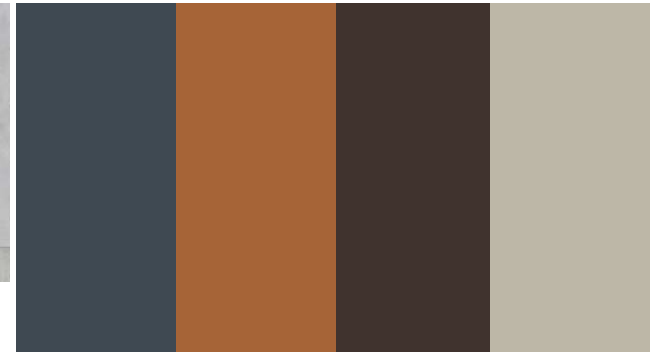
4. Cementitious Panel Siding



5. Perforated  
Aluminum Screen



6. Architectural  
Concrete



A. Blue Note  
BM2129-30

B. Copper Mountain  
SW6356

C. Black Bean  
SW6006

D. Analytical Gray  
SW7051



GATEWAY SOUTH ELEVATION



GATEWAY EAST ELEVATION



GATEWAY – LOOKING NORTH UP 108TH AVENUE



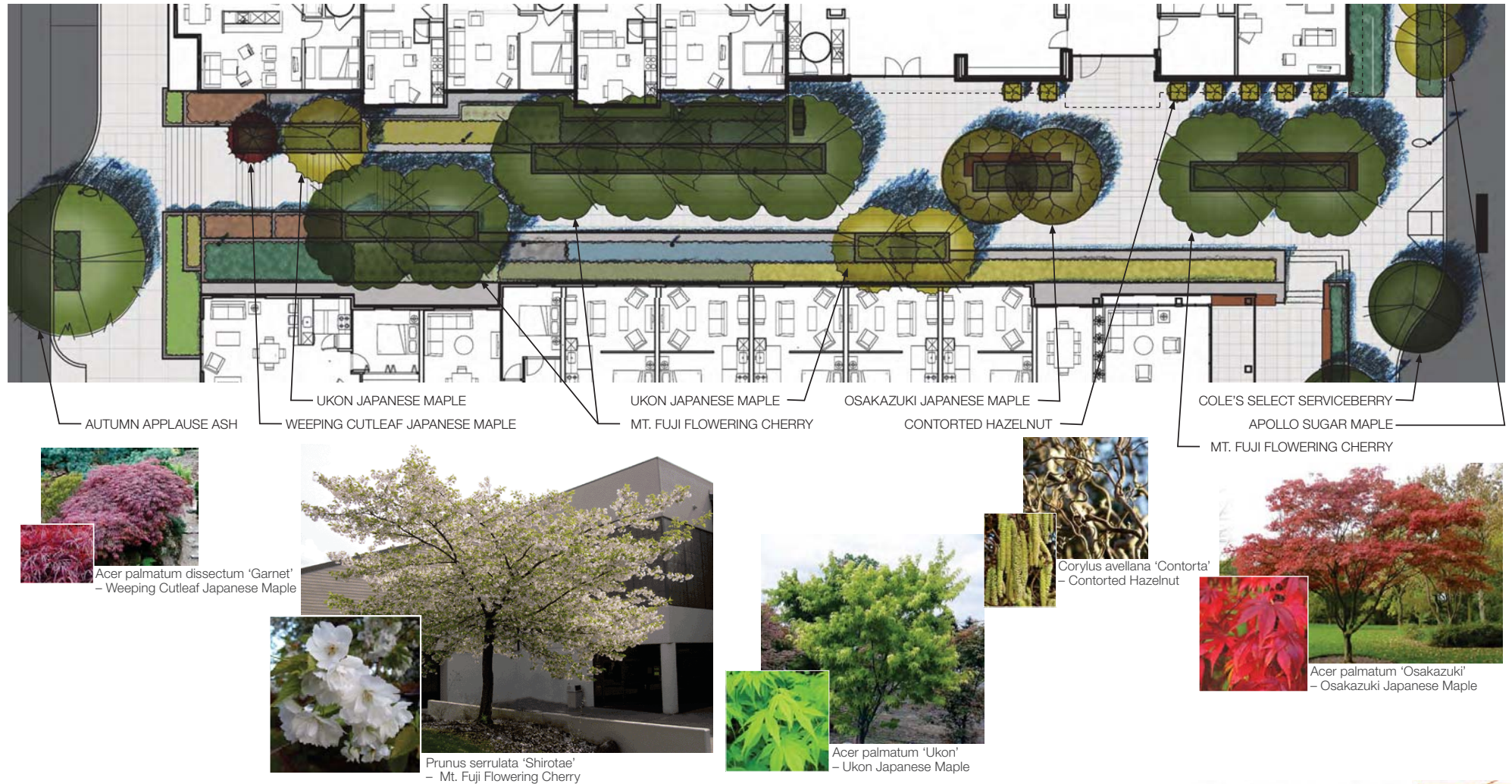
GATEWAY – LOOKING WEST DOWN 108TH AVENUE

# MIDBLOCK PEDESTRIAN PLAZA

LANDSCAPE DETAILS



# MIDBLOCK PEDESTRIAN PLAZA TREES



POLYGON NORTHWEST



KIRKLAND TOD | MIDBLOCK PEDESTRIAN PLAZA LANDSCAPE



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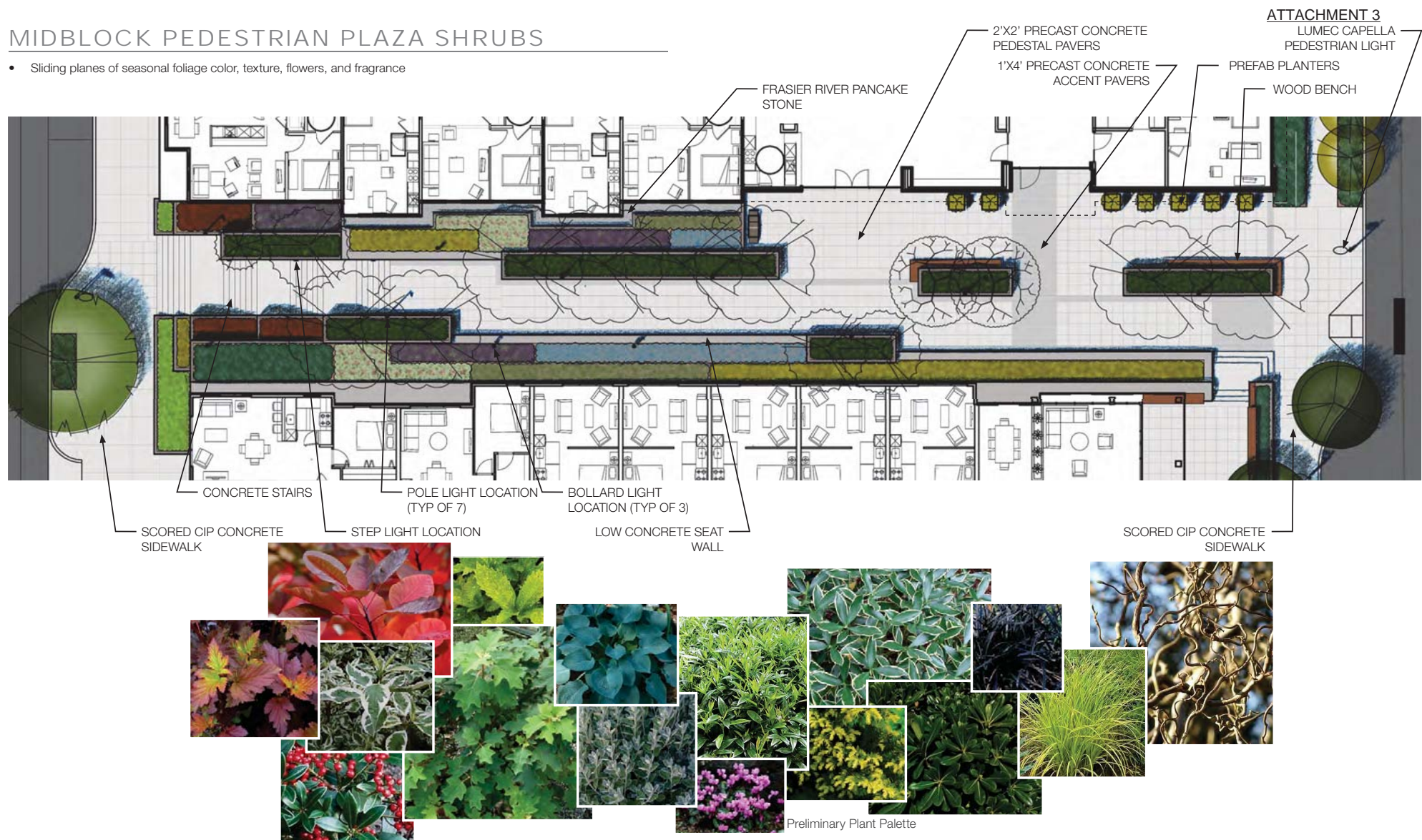
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## MIDBLOCK PEDESTRIAN PLAZA SHRUBS

- Sliding planes of seasonal foliage color, texture, flowers, and fragrance



POLYGON NORTHWEST

**Imagine Housing**  
building eastside communities

KIRKLAND TOD | MIDBLOCK PEDESTRIAN PLAZA LANDSCAPE

**SMR**  
ARCHITECTS

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LOOKING WEST AT LOBBY ENTRANCES



LOOKING EAST FROM 38TH PLACE

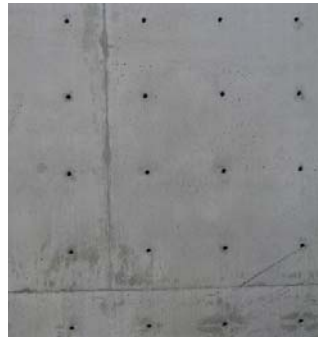


PEDESTAL PAVERS



PEDESTAL PAVER DETAIL

ACCENT PAVER DETAIL



CONCRETE PAVERS



PED LIGHT (8')



BOLLARD LIGHT (39")



STEP LIGHT

POLYGON NORTHWEST



KIRKLAND TOD | MIDBLOCK PEDESTRIAN PLAZA

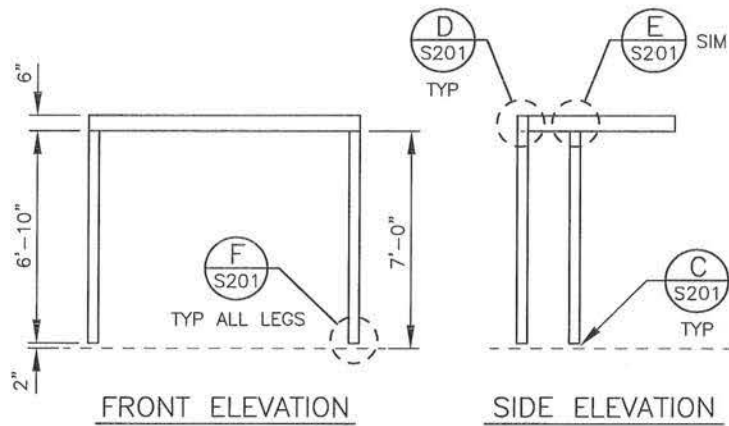
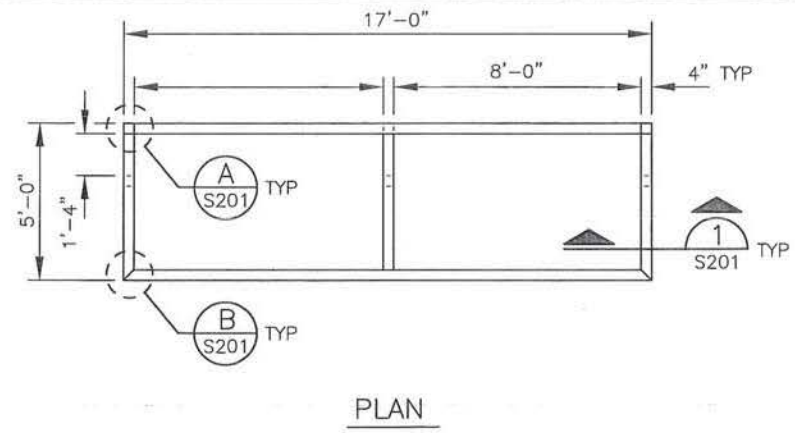
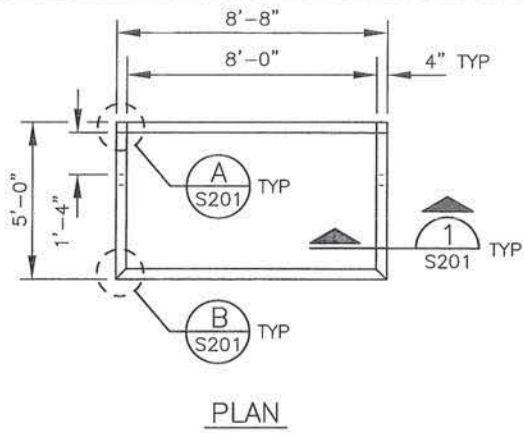


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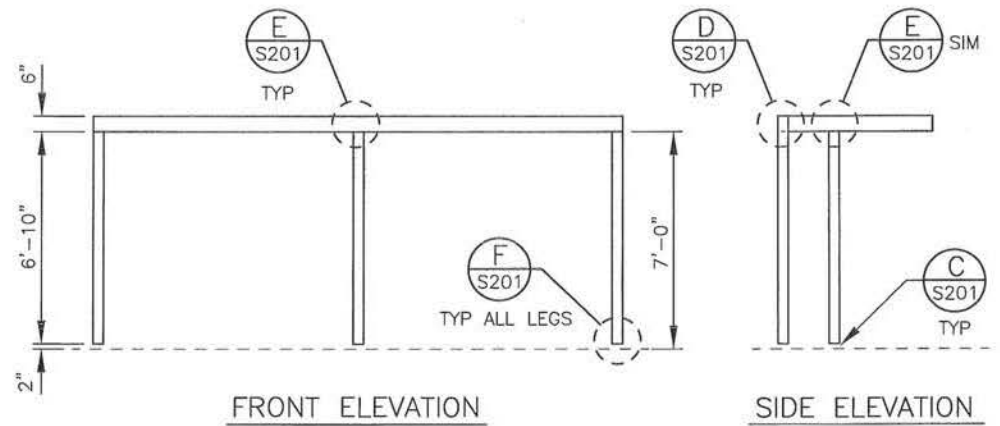


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F51-FRAME



F52-FRAME



EXPIRES 5/25/ 07



King County

DESIGNED PE		Department of Transportation - Transit Division		DATE: JAN 06
DRA: LSA	CHKD:	SCALE: 1/4"=1'-0"	FABRICATION AND DELIVERY OF TRANSIT PASSENGER SHELTER FRAMES FRAME - SERIES 50 (F51 & F52)	FILE
RECOM. PE	APPROVED	CONTRACT: 06-002 MM		DWG. NO. S106
				SHT 7 OF 11